



ERIE CANAL HARBOR DEVELOPMENT CORPORATION

at the offices of
Empire State Development – Buffalo Regional Office
95 Perry Street, Suite 500
Buffalo, NY 14203

Meeting of the Directors

Tuesday
February 14, 2017 – 10:30 AM

PROPOSED AGENDA

CORPORATE ACTION

1. Approval of the Minutes of the October 11, 2016 Directors' Meeting
2. Procurement of Consulting Services - Authorization to Enter into an Agreement for Consulting Services Regarding Insurance Matters and to Take Related Actions
3. Children's Museum - South Aud Block Redevelopment (130 Main Street) - Design Approval of Architectural Design; and Authorization to Take Related Actions

FOR INFORMATION

4. President's Report – Oral Report
5. Phoenix Award – Oral Report

Item #1

Erie Canal Harbor Development Corporation
Meeting of the Directors
Held at the Offices of Empire State Development
– Buffalo Regional Office
95 Perry Street – Suite 500
Buffalo, New York 14203

October 11, 2016

MINUTES

In Attendance

Directors:

Robert D. Gioia – Chairman
Julie Bargnesi
Michael DiVirgilio
Sam Hoyt (via videoconference)
Makau W. Mututa
County Executive Mark C. Plonocarz
Mayor Byron W. Brown (Non-Voting Director)

ECHDC Staff:

Kaitlin Chmura, Project Manager
Jill Clark, Assistant Project Manager
Debbie Royce, Corporate Secretary (via videoconference)
Bryan Watson, Executive Assistant

ESD Staff:

(Attending via
video conference
unless otherwise noted)

Julia Borukhov, Financial Project Manager
Thomas Brennan, SVP - Internal Audit
Stephen Gawlik, ESD - VP Capital Projects & Senior Counsel and
ECHDC - Assistant Secretary (attending in Buffalo)
Kathleen Mize, Deputy Chief Financial Officer and Controller
Max Padden, Director, Subsidiary Finance
Robert White, AVP - Internal Audit

Also Present:

(Attending in Buffalo)

Thomas Hersey – Commissioner, Erie County Department of
Environment and Planning
Jim Fink, Business First
Kathleen Peterson – City of Buffalo – Office of Strategic Planning
Members of the Media
Members of the Public

The meeting of the Erie Canal Harbor Development Corporation (the “Corporation”),
was called to order at 10:50 a.m. It was noted for the record that the meeting was being

webcast, and that the Directors had received all written material in advance of the meeting. Chairman Gioia then noted for the record the Corporation's policy which welcomes public comments on the items on the current Agenda.

Before beginning with the substantive portion of the meeting, Chairman Gioia asked the Directors whether anyone had any potential conflict of interest with respect to any of the items on the proposed Agenda. Hearing none, Chairman Gioia then called upon Kaitlin Chmura to present a request for authorization for the Corporation to enter into a contract for the design and fabrication services for the Canalside Wayfinding & Interpretive Signage Project.

Ms. Chmura noted that the contractor would be Entro Communications and it would be funded by a New York Power Authority Industrial Incentive Award. She note that following the Summer of 2016 season which was the most successful to date, it was evident by the size and consistency of the crowds that wayfinding and interpretive signage is needed in order to connect and showcase all of the assets that Canalside has to offer the public. She further noted that the signage would not only help the public navigate the environment, but it would also help to educate the visitors of the historical significance of the district as it not only related to the Erie Canal but how it also related to the role it played in United States history.

Ms. Chmura explained that in June 2016, the Corporation issued a Request for Qualifications in the *New York State Contract Reporter* and posted it on its website. She advised that eight responses were received and the review committee short-listed three for

interview. She stated that based on the interview process, two companies were then invited to respond to the Request for Proposal.

Ms. Chmura advised that Entro Communications was the highest ranked team throughout the entire procurement process. She further advised that Crystal McKenzie, Inc. will develop the mobile app and Design Communications Limited is the fabrication lead and Signs and Decals will provide the fabrication and installation services. She noted that there is a 30% M/WBE goal for the contract and that Crystal McKenzie, Inc. and Signs and Decals are MBE contractors for the Project.

Ms. Chmura noted that upon completion of the design phases, the Corporation would review the program components before authorizing the team to begin the fabrication phase. She also noted the involvement of the Canalside History Advisory Group which is made up of volunteers who are local historians who were tasked with developing the interpretive content for the Project to ensure that the stories being told were accurate.

Director Hoyt noted that he was particularly excited about the historic interpretation and interactive components of the Project. He explained that emphasizing and celebrating the history at Canalside is something that needs improvement and that these components would allow that to happen. He also noted that a mobile app is a great opportunity to better inform visitors as to what is happening on a daily basis.

Mayor Brown then expressed his support for the installation of the wayfinding and signage and wanted to thank the staff for their hard work that was put into this procurement. He asked whether the signage would also direct the public to another immediate area of Canalside, the Naval and Military Park and Erie Basin Marina. Ms. Chmura stated that it is not considered in this scope of the contract. Mayor Brown also stated that he was please to see that there was a 30% M/WBE goal connected with the contract. Lastly, Mayor Brown stated that he thought the Project would also help with traffic circulation and the crowds of people being able to better find their way more efficiently.

Commissioner Hersey then noted the County's support for the approval of the contract and asked that the consideration be made to try to keep a regional consistency in the signage by taking a look at the conventions and standards used in the signage for the shoreline area and trails.

Chairman Gioia then noted that he would think that the process will inventory all of the signage in the area and try to clarify it so that it can be easily read and people can navigate and maneuver around easily.

Hearing no further questions or comments from the Directors and no comments from the public, upon motion duly made and seconded, the following resolution was unanimously adopted:

ERIE CANAL HARBOR DEVELOPMENT CORPORATION – Canalside Wayfinding & Interpretive Signage Project - Design and Fabrication Services - Authorization to Enter into a Contract for Design and Fabrication Services of the Canalside Wayfinding & Interpretive Signage Project; Authorization to Take Related Actions

BE IT RESOLVED, based on the materials at this meeting (the “Materials”), a copy of which is hereby ordered filed with the records of the Corporation, the Corporation hereby finds the Entro Communications, Inc. to be responsible; and be it further

RESOLVED, that the Corporation is hereby authorized to enter into a contract with Entro Communications, Inc. for a contract amount not to exceed ONE MILLION ONE HUNDRED FIFTY THOUSAND DOLLARS (\$1,150,000) (\$1,000,000 plus \$150,000 contingency) for the purposes and services, and substantially on the terms and conditions, set forth in the materials; and be it further

RESOLVED, that the President of the Corporation or his designee be, and each of them hereby is, authorized to take such action and execute such documents as may be necessary or appropriate to carry out the foregoing resolution.

* * *

Hearing no further business, the meeting adjourned at 11:10 a.m.

Respectfully submitted,

Debbie Royce
Corporate Secretary

Item #2



FOR CONSIDERATION

February 14, 2017

TO: The Directors

FROM: Thomas P. Dee

SUBJECT: Procurement of Consulting Services

REQUEST FOR: Authorization to Enter into an Agreement for Consulting Services
Regarding Insurance Matters and to Take Related Actions

CONTRACT NEED AND JUSTIFICATION

I. **Project Summary**

Recipient: Keville Insurance Agency (the "Contractor")
Waterford, NY 12188

Scope of Services: Provide consulting services with respect to insurance matters
effecting Erie Canal Harbor Development Corporation ("ECHDC").

Contract Term: Three years (3/2017 through 3/2020)

Contract Amount: Contract amount: \$45,000

Funding Source: NYPA Relicensing Settlement Agreement

II. **Background**

In August of 2015, ECHDC entered into a one year agreement with the Contractor to provide consulting services with respect to ECHDC's insurance needs. As the agreement did not exceed \$50,000 nor did the term extend beyond one year, no Board approval was required. In 2016 as the contract was going to expire, the ECHDC Board extended the original contract another year. Over the past two years, the Contractor has provided valuable input and advice to ECHDC regarding its insurance needs, including but not limited to the types and limits needed for ECHDC's various projects. In not only ensuring that ECHDC has the appropriate level of insurance for its various needs at the best value, the Contractor has been able to work with various insurance providers to obtain significant savings to ECHDC, notably \$100,000 on the Grain Elevator Illumination project, and \$121,000 on insuring Terminal A & B.

The principal of the Consultant is Rose Keville. Ms. Keville has considerable experience in the insurance industry with over 25 years of experience in all lines of property and casualty risk management and insurance placement services and of that, 20 years providing assistance to ESD in one capacity or another. As former Director of the Bureau of Risk and Insurance Management for the State of New York, Ms. Keville has extensive experience in placing large property and casualty programs for the State as well as for public benefit corporations and public authorities such as the State University Construction Fund, Jacob Javit's Convention Center, the New York State Thruway Authority and ESD. In addition, Ms. Keville was a member of the World Trade Center Insurance Task force and responsible for the placement of the wrap up insurance program for the WTC Debris Removal Project.

Considering the wide range of projects ECHDC is engaged in, and its increased property portfolio, the services provide valuable protection to ECHDC through its insurance needs. As such, the Board is requested to approve a new contract through March 2020 in the amount of \$45,000 (effective March 1, 2017).

III. Contractor Selection Process

Pursuant to the ESD/ECHDC procurement guidelines adopted by this Board, services from small business concerns or those certified pursuant NYS Executive Law Article 15-A may be retained without a formal procurement process for contracts that do not exceed \$200,000. As the Contractor is a certified Women's Business Enterprise pursuant to Article 15-A and due to Contractor's previous work with ESD and ECHDC and familiarity with ESD/ECHDC insurance requirements, no additional quotes were sought for the services requested.

Pursuant to State Finance Law Section 139-j and 139-k and the Corporation's policy related thereto, staff has; a) considered proposed contractor's ability to perform the services provided for in the proposed contract; and b) consulted the list of offerers determined to be non-responsible bidders and debarred offerers maintained by the New York State Office of General Services. Based on the foregoing, staff considers the proposed contractor to be responsible.

IV. Scope of Work

The Contractor will provide advice to ECHDC as needed regarding all insurance matters regarding the Corporation. In addition, the Consultant will act as the Corporation's liaison with ESD's insurance broker to identify the appropriate insurance requirements necessary for ECHDC's projects.

V. Terms and Conditions

The Contractor shall be paid on a time and material basis. The contact terms shall be three years effective march 1, 2017 in an amount not to exceed \$45,000 (\$15,000 per year).

VI. Non-Discrimination & Contractor and Supplier Diversity

MINORITY AND WOMEN-OWNED BUSINESSES

ESD's Minority and Women-owned Business Enterprise ("MWBE") policies will apply to this contract. The Contractor shall be required to solicit and utilize MWBEs in the fulfillment of the requirements of this contract. The Contractor must demonstrate the use of good faith efforts pursuant to 5 NYCRR §142.8 to achieve an overall goal of 30% for MWBE Participation.

SERVICE-DISABLED VETERAN-OWNED BUSINESSES

ESD's Service-Disabled Veteran-Owned Business ("SDVOB") policies will apply to this Contract. ESD conducted a comprehensive search and determined that the Contract does not offer sufficient opportunities to set specific goals for participation by SDVOBs as subcontractors, service providers, and suppliers to the Contractor. Nevertheless, the Contractor is encouraged to make good faith efforts to promote and assist in the participation of SDVOBs on the Contract for the provision of services and materials.

VII. Environmental Review

ESD staff has determined that the requested authorization constitutes a Type II action as defined by the New York State Environmental Quality Review Act and the implementing regulations of the New York State Department of Environmental Conservation. No further environmental review is required in connection with the authorization.

VIII. Requested Action

The Directors are requested to (1) make a determination of responsibility with respect to the proposed Contractor; (2) authorize the Corporation to enter into an agreement with the Contractor on the terms contained in these materials and (3) take all related actions.

IX. Recommendation

Based on the foregoing, I recommend approval of the requested actions.

X. Attachment

Resolution

February 14, 2017

ERIE CANAL HARBOR DEVELOPMENT CORPORATION – Procurement of Consulting Services -
Authorization to Enter into an Agreement for Consulting Services Regarding Insurance Matters
and to Take Related Actions

BE IT RESOLVED, that upon the basis of the materials presented to this meeting (the "Materials"), a copy of which is hereby ordered filed with the records of the Erie Canal Harbor Development Corporation (the "Corporation"), the Corporation hereby finds the Keville Insurance Agency (the "Contractor") to be responsible; and be it further

RESOLVED, that the Corporation is hereby authorized to amend a contract with the Contractor in an amount not to exceed Forty Five Thousand Dollars (\$45,000) for a three year term effective March 1, 2017, for the purposes and services, and substantially on the terms and conditions, set forth in the Materials; and be it further

RESOLVED, that the President of the Corporation or his designee be, and each of them hereby is, authorized to take such action and execute such documents as may be necessary or appropriate to carry out the foregoing resolution.

* * *

Item #3



FOR CONSIDERATION

February 14, 2017

TO: The Directors

FROM: Thomas P. Dee

SUBJECT: Children's Museum - South Aud Block Redevelopment (130 Main Street)

REQUEST FOR: Design Approval of Architectural Design; and Authorization to Take
Related Actions

I. Background

The former Memorial Auditorium Block ("Aud Block") was acquired by the ECHDC in 2008 and since that time, ECHDC has invested public funds remediating hazardous waste, lead and asbestos containing materials, completing demolition of the Memorial Auditorium, relocating utilities and preparing the site for future development. Through a local planning process, the Aud Block has been divided into multiple parcels reflecting the 19th century streets/building typology of the site, including the "Canals and Public Environments", or public space, designed as an interpretive landscape of water features and site amenities that will help visitors understand the history and character of the former canal district and its significance to the City, New York State and the nation.

On October 9, 2013, the Erie Canal Harbor Development Corporation ("ECHDC" or the "Corporation") Board of Directors authorized the Corporation to accept the Explore & More Children's Museum's ("EMCM") Master Plan and Pre-Campaign Capital Feasibility study and enter a Non-Binding Memorandum of Understanding to address the roles and responsibilities of ECHDC and Explore & More Children's Museum as the project proceeded, including the building development schedule, funding requirements, the museum fundraising timeline, and other project-related milestones. Further Board approval was articulated prior to the execution of any binding agreements or the expenditure of funds by ECHDC to move the project forward.

In February 2014, the ECHDC retained the services of Fontanese, Folts, Aurbrecht & Ernst ("FFAE") team to complete detailed architectural and engineering designs and the preparation of the construction bid package, including all plans, proposals, specifications, estimates, notes, special contract requirements, and any other contract documents necessary to advance the core and shell building to construction, as well as construction administration and commissioning. The FFAE team is the "architect of record" under the direction of Ehrenkrantz, Eckstut & Kuhn, a Perkins Eastman Company, the "master architect".

The ECHDC and Explore & More collaborated on a museum design which was a component of a much broader, three-parcel South Aud Block development project. The design progressed through Schematic Design with an economic analysis being developed for the project. Eventually these documents became part of a private redevelopment RFP effort that was not successful. After reconsidering the scope and scale of the project in 2015, the ECHDC and Explore & More Children's Museum determined that proceeding with a standalone museum project would best fulfill each partner's goals and objectives. Although construction documents would be finalized in the future, it was assumed that this standalone facility would be located on Parcels A2.1, A2.4, and a section of "Lake Street" located on the South Aud Block.

As a result, on September 16, 2015, the ECHDC Board authorized the Corporation to enter into the necessary agreements with EMCM to design and construct a nearly 43,000 GSF core & shell structure on the South Aud Block. This structure will accommodate the Children's Museum on five levels with an outdoor terrace. The building has a floorplate of approximately 9,000 GSF with access from the towpath and street levels. The sub-basement from the former Memorial Auditorium is located underneath most of the building and shall continue to be used as the mechanical equipment room for the public canals, as well as for the new museum. The terms of the deal with EMCM require ECHDC to design the core and shell of the structure and then proceed with construction. The design is now complete with construction set to begin in March 2017. LeChase Construction, as authorized by the ECHDC Board on May 9, 2016, will provide construction management services for the pre-construction and construction of the Children's Museum core and shell structure.

II. Description of Project Design

The Project involve a \$13.29 million, 43,000 GSF core and shell building that will house the Explore & More Children's Museum and provide a number of exhibits that would further enhance Canalside's status as the City's premiere downtown location.

The lower floors would be activated with a café along the Canal Towpath and a retail space on the ground floor along Lloyd Street, anchoring the corner of Lloyd Street and Marine Drive. The ground floor also includes two entrances for Museum guests, allowing school groups to be separated from daily visitors as necessary. The second floor includes a two-story space along the canals while the third floor provides space for several exhibits and views of the Buffalo River. On the fourth floor, overlooking the canals and with the City skyline as backdrop, a rooftop terrace will allow Museum guests to enjoy Canalside from a unique vantage point.

The exterior of the building is generally clad in brick with a stone base along the towpath. The brickwork and windows on the majority of the building are reminiscent of a mid-19th century canal-era building, while contemporary metal panels and windows along the southern edge highlight "the cut" through the building to accommodate today's site features. The retail front extends the length of Lloyd Street with opportunities to "look into" the Museum from the outside.

The proposed Project will strive to achieve Leadership in Energy and Environmental Design (LEED) Silver based on the LEED for Core & Shell Construction.

Renderings and construction drawings of the Project are attached hereto as **Exhibit A**.

III. Compliance of Project with MGPP Objectives and Canalside Design Guidelines

The Project has been formally reviewed by the Canalside Design Review Committee (the "DRC") to determine its consistency with the Canalside Design Guidelines. After their technical review, the DRC convened and unanimously recommended that ECHDC approve the Project Plans. The DRC did have several comments regarding exterior finishes, building lighting and signage. A copy of the DRC's recommendations is attached hereto as **Exhibit B**. The ECHDC and the Museum (for signage) will respond to the open items listed in the DRC's recommendation and will schedule additional meetings with the DRC as appropriate to address the above items.

The Project plans were also referred to the City of Buffalo Planning Board for a public hearing and consideration on their consistency with the Design Guidelines prior to ECHDC accepting the plans as complete and authorizing construction. The Planning Board held its public hearing on February 13, 2017. The findings of the public hearing will be presented at the Board Meeting. Based upon review and consideration of both the DRC's and Planning Board's recommendations, ECHDC staff finds that the Project is consistent with the Canalside Design Guidelines.

IV. Environmental Review

ESD, as lead agency, completed an environmental review of the Canalside Project, which included development assumptions for the South Aud Block, pursuant to the requirements of the State Environmental Quality Review Act ("SEQRA") and the implementing regulations of the New York State Department of Environmental Conservation. This review, which was coordinated with involved agencies, including ECHDC, due to the Project's Type I classification, included the preparation of a Draft and Final Generic Environmental Impact Statement ("GEIS"). The ESD Board of Directors issued SEQRA findings for the Canalside Project on March 13, 2010. Because the proposed scale/uses associated with a proposed standalone facility to house the Explore & More Museum are well within the thresholds established for the South Aud Block in the Canalside Project Final GEIS and SEQRA findings, no further environmental review is required in connection with this action.

V. Recommendation

Based upon the foregoing, the Directors are requested to: 1) based on the recommendations of the ECHDC's Design Committee and the City of Buffalo Planning Board, find that the Project is consistent with the Design Guidelines established by the Corporation as part of the MGPP for the Canalside Land Use Improvement Project without modification thereof; and 2) take all related actions.

Attachments

Resolution

Exhibit A – Project Renderings and Construction Drawings (Floorplans/Elevations)

Exhibit B – Canalside Design Review Committee comments

February 14, 2017

ERIE CANAL HARBOR DEVELOPMENT CORPORATION – Children’s Museum - South Aud Block
Redevelopment (130 Main Street, Buffalo, New York) - Design Approval of Architectural Design;
and Authorization to Take Related Actions

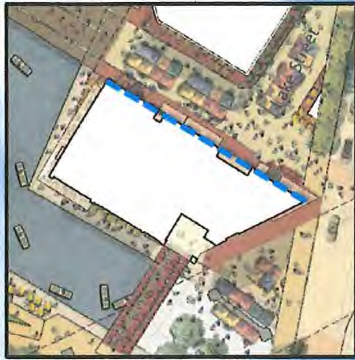
RESOLVED, that on the basis of the materials presented to this meeting, a copy of which is hereby ordered filed with the records of the Erie Canal Harbor Development Corporation (the “Corporation”), and based on the recommendations of the Corporation’s Design Review Committee and the City of Buffalo Planning Board, the Children’s Museum – South Aud Block Redevelopment (“Project”) is found, without modification thereof, to be consistent with Design Guidelines established by the Corporation as part of the Modified General Project Plan for the Canalside Land Use Improvement Project; and be it further

RESOLVED, that the Chairman and President or his designee(s), be and the same hereby is, authorized in the name of and on behalf of the Corporation to take such actions as may be considered necessary or appropriate to give effect to this Resolution.

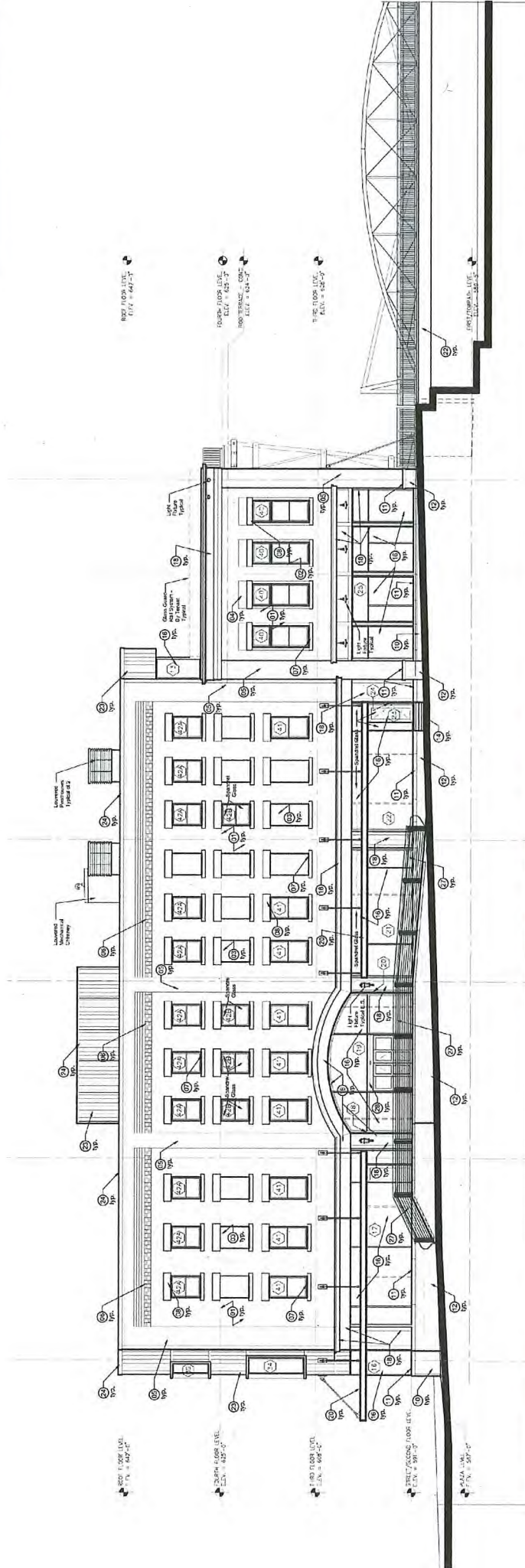
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Exhibit A

East Exterior Elevation



**FONTANESE
FOLTS
AUBRECHT
ERNST
ARCHITECTS**



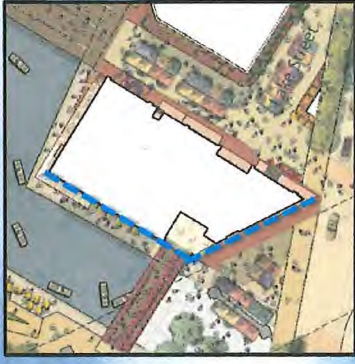
NEW YORK
STATE OF
OPPORTUNITY.

FFAE Architects, P.C. - COPYRIGHT 2016

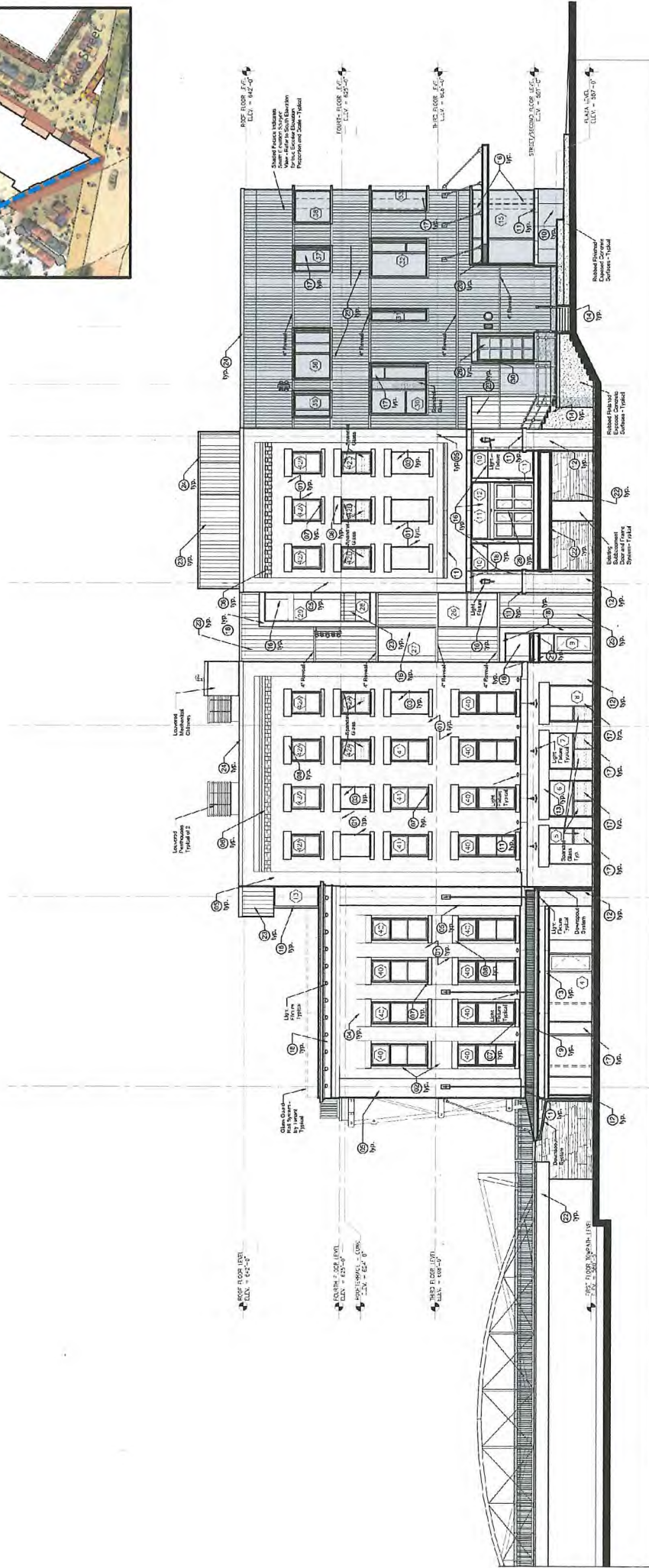
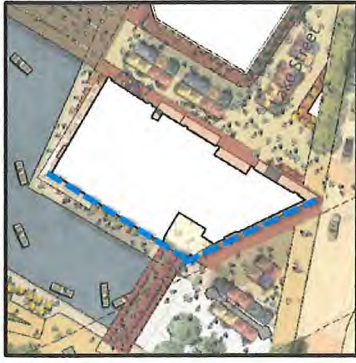


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West & South (Splayed) - Exterior Elevation(s)

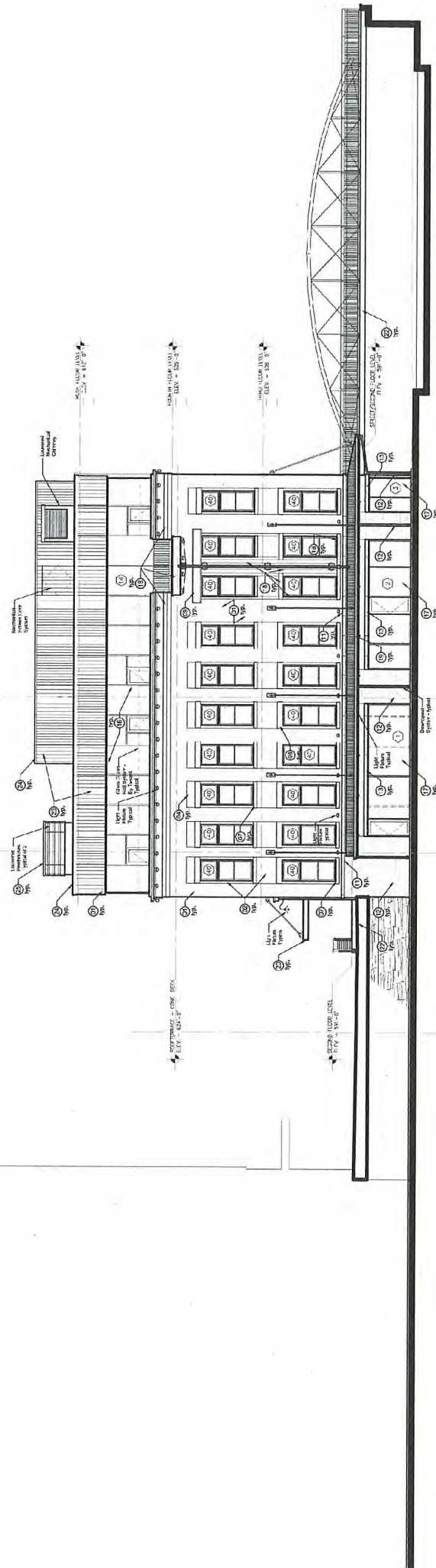


West & South (Splayed) - Exterior Elevation(s)

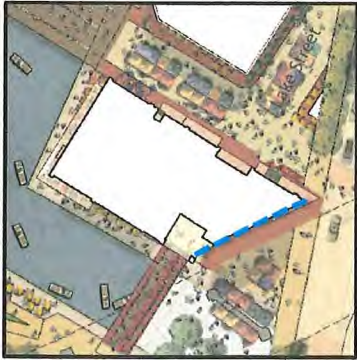


North Exterior Elevation

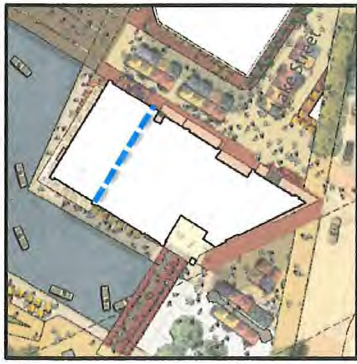




South & North Roof Terrace - Exterior Elevation(s)



South Exterior Elevation



North Roof Terrace
Exterior Elevation

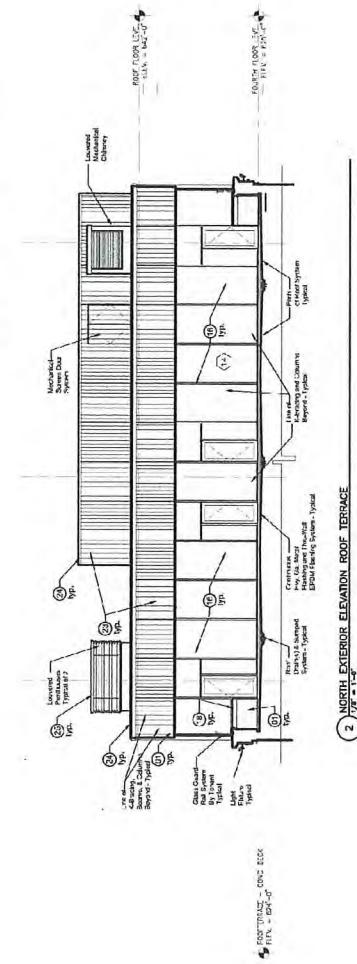


Exhibit B

Erie Canal Harbor Development Corporation
Design Review Committee
February 8, 2017

Thomas P. Dee
President
Erie Canal Harbor Development Corporation
95 Perry Street / Suite 500
Buffalo, New York 14203

Design Review Committee (DRC)
South Aud Block Redevelopment • Design Review Recommendations

Dear Tom,

Below please find the Design Review Committee's comments based on the South Aud Block Redevelopment Submission dated 1-20-2017. As with previous submissions, this committee has completed a thorough review of the design documents provided and based all comments based on the ECHDC and EEK canal side design guidelines dated July 2009 (revised March 2010).

Unfortunately, this particular Parcel falls within the Aud block and is not specifically addressed in any of the site plan exhibits included in the design guidelines. Although not addressed in the guidelines, the committee took the liberty to apply the guidelines as necessary.

Many of the comments below speak directly to conformance to guidelines and in some cases the lack of information to evaluate the proposed facility based guidelines. However, at this time this committee recommends approval based on the following comments and modifications:

Use.

- No comment.

Active Ground Floor uses

- Though there is active ground floor uses along Lloyd Street and the elevations which about the canal, there is no active ground floor uses on the marine drive/ south facing side of the facility. The NW corner of the tow path could be the most important corner at Canal side. It will set the example for all other development. Seems like the floor to floor height at this level is a little compressed...??? Be very careful about the doors swinging into a very busy corner / tow path.

Massing

- *The intent of the guidelines is to avoid creating an inauthentic facsimile of the historic urban form - instead the intent is to develop characteristics of historic precedents at Canal Side.* Lloyd Street Front Door Entrance 01, is not as active and welcoming as Front Door 02 on the Mercantile Bridge elevation.
- Note later comments on the rooftop skyline. The arch on the Lloyd street side is weak because it is flat against the building. It is recommended that this projects out to at least the line of the canopies for weather protection for the children using this facility.

Build-to Lines

- There is a large setback on the Marine Drive side of the facility. However, this parcel is not specifically addressed in the design guild lines. The Lloyd street side and canal side of the facility have great street walls.
- See comments regarding Streetscape.

Height Limits

- The facility is approximately 60' high and is within the height limit of other parcels along marine drive.

Circulation

- Circulation within the building appears very cut up by what appears to be diagonal bracing and the bracing would be very unfortunate in front of the glass wall to the roof terrace.
- Is diagonal bracing the only way to stiffen the structure?
- It is our recommendation that the base of the Entrance 01 pushes to reflect either historic precedent or a playful "contemporary" interpretation of an industrial mercantile vocabulary. The design team recommends further inquiry in the design of exterior lighting to reflect the same.

Streetscape

- There is not enough detail to comment of streetscape materials. Approval should be contingent of a future review of streetscape materials.
- Sheet C012 - Not sure what the base conditions are for curbs, sidewalk, or road, but we suggest the granite curb, exposed medina aggregate, sandstone pavers, & sandstone cobble pavers for Lloyd Street.

- Why brushed concrete stairs? We suggest using the exposed aggregate.
- No indication of stair riser & tread dimensions. On these exterior stairs I would strictly enforce a maximum rise of 6".
- A101 Tow Path Plan - watch for door swings into a very busy corner. It could be the busiest corners at Canal side.
- West Elevation – The canopy should extend another bay to the south.
- Guidelines call for a two story base.
- Please provide sections A-306, 1-A300, 1-A314 to fully understand the buildings edge at the canal.

Parking

- N/A

Structural parking

- N/A

Access and entrances

- Need better definition of curbing & cuts on south side of building.

Refuse collection

- The general service street is along Marine Drive per the guidelines. There is already service entry located at harbor center along Marine Drive, so there is precedent for this.
- All trash stored in building & taken outside the morning of pick up. Make every attempt to not have trash sitting outside over night.

Architectural features

- The metal composite paneling shown along the Lloyd street entrance will look very corporate and does not reflect the historic nature of the area.
- From our understanding the metal composite arch of the main entry, doesn't allow coverage from rain/snow/sun while groups are gathering to enter the building. A covered awning would also serve as a visual cue.
- Light fixtures at both entrances should be bold & very special, and much larger.
- The south elevation with the ribbed metal is needs improvement. The window pattern is neither historic nor contemporary. This would have been a perfect opportunity for a very glassy elevation showing off the guts of the building.

Edges

- How is the sharp corner of the building along Maine Drive detailed? Please provide plan details at 1"=1' scale

Bases

- We would like to see more detail in the base of the building on the Lloyd street elevation. There is an opportunity for the history of the area to be introduced at this level.
- Guidelines call for a two story base.

Storefront and Retail facades

- There is not enough detail provided to evaluate. The design of the exterior doors should be special. Appear to standard models from a suburban drug store. How do these contribute to the character of Canalside?
- No vestibule or air-lock at the two main entrances?

Corners

- How is the sharp corner of the building detailed? This might be the ideal place for a large vertical sign the height of the building elevation. **The Guideline** "Building Corners provide an opportunity for establishing architectural emphasis. Perhaps this is why the graphic signage on the Adaptive re-use addition facade is shown by the museum. Suggestion: The architecture wraps the corner.

Cornices

- The main building loses its cornices for a metal coping which seems to be an afterthought. The main building cornices should be more pronounced.
- Not sure why the north building has a metal cornice. Not enough detail to evaluate this. We enjoy the corbeled simpler brick cornice but object to the aluminum roof edge coping. Why not a clay camelback coping?
- The metal panels above the glazing to the roof terrace are weak.
- Light fixtures on the metal cornice? No detail & what are they illuminating? Will these cause glare, hot spots, & general light pollution?
- Material no. 24 - metal coping / fascia is inappropriate for this building. Clay camelback would finish it beautifully. The coping appears oversized and the usually show oil canning.

Canopies

- Same note as above: From our understanding the metal composite arch of the main entry, doesn't allow coverage from rain/snow/sun while groups are gathering to enter the building. A covered awning would also serve as a visual cue.

- Not enough detail to evaluate. Why three different types of canopies? Standing seam, sunshade type (does this provide weather protection?)
- And a glass system. Details would be helpful to evaluate.

Skyline

- Visual interest is needed at the rooftop plan to fully comply with the design guidelines.

Materials

- The metal composite paneling shown along the Lloyd street entrance will look very corporate and does not reflect the historic nature of the area. The color will be very important.

Building Materials and color.

- Material 07 & 08, stone headers & lug sills should be real cut stone & not imitation materials. The brick system, while I consider it questionable, it is at least a real clay product.
- Recommend a dark painted color on the punched windows. Is there rowlock coursing above the cast stone headers? It is usually one of the other? This is on the north face of the building.

Glass and fenestration

- The DRC needs to see a sample of the spandrels glass that is proposed on the east elevation.
- The design guidelines require the building glass be clear. The glass is clear, but is then treated with a metallic low-e coating, which helps energy performance, but in my opinion reduces the light transmittance value, but more importantly adds to the reflectivity of the glass from the outside during the day, and adds to the reflectivity at night from the inside. The intent of the guidelines is to promote TRANSPARENCY. A good mock up might prove otherwise to the DRC.
- Note that the colored rendered elevations do not match the technical elevations.

Signage

- This approval would be contingent of a future review of proposed signage. The west and south elevations lacked the visual delight and invention mentioned in the design guidelines. The entrance at door 01 also lacked the visual delight. As mentioned above: the corner on Marine Drive might be a place for for a large vertical sign the height of the building elevation.

Lighting

- This approval would be contingent of a future review of proposed lighting.
- The choice of lighting could be more playful or historic. As mentioned above "historic precedent" or a playful "contemporary" interpretation of an industrial mercantile vocabulary would offer a stronger design solution. Perhaps the choice of the fixture is oversized rather than undersized.

- Not nearly enough information to evaluate. Down lighting should be used sparingly & with effective glare control.
- Previous reviews included a presentation from a lighting consultant. Very important to provide a cohesive feel & character of lighting for all of Canalside. Not always in terms of fixture type, but color rendering index & color temperature in degrees Kelvin.

Sustainability guidelines

- No comment.

Energy

- The metal ribbed paneling on the south side of the facility will absorb a lot of heat. Has the use of this paneling during the summer months been taken into consideration? Further attention is needed regarding the color of this paneling.
- No vestibule or air-lock at the two main entrances?

Indoor environmental quality

- There is not enough detail provided to comment on this.

Water Management

- There is not enough detail provided to comment on this.

Outdoor environment and site construction

- There is not enough detail provided to comment on this.

Sincerely,

Rishawn Sonubi

Max Willing

Audrey Ross Sanders

Erie Canal Harbor Center Corp Design Review Committee

February 9, 2017

Attention: Mr. Thomas P. Dee, President
Erie Canal Harbor Development Corporation
Attention: South Aud Block A/E Design Services
95 Perry Street, Suite 500
Buffalo, NY 14203

**Re: Echdc's Drc Review Recommendation Response Letter:
South Aud Block Redevelopment
Canal Side Project, Buffalo NY**

Dear Mr. Dee,

Fontanese Folts Aubrecht Ernst Architects, P.C. is pleased to submit collective response(s) to the Erie Canal Harbor Development Corporation's Design Review Committee's (DRC) recommendations and commentary based on the latest and final submission for the redevelopment of the South Auditorium Block at Canalside – Children's Museum. We have itemized our response commensurable to the DRC's commentary as follows (following in "blue" indented hollow bulleted descriptions):

Use.

- No comment.

Active Ground Floor uses

- Though there is active ground floor uses along Lloyd Street and the elevations which about the canal, there is no active ground floor uses on the marine drive/ south facing side of the facility. The NW corner of the tow path could be the most important corner at Canal side. It will set the example for all other development. Seems like the floor to floor height at this level is a little compressed...??? Be very careful about the doors swinging into a very busy corner / tow path.
 - THE RETAIL SPACE ALONG THE EAST FAÇADE WRAPS THE CORNER ONTO THE SOUTH FAÇADE. THE REST OF THE SOUTH FACE IS DEDICATED TO REFUSE COLLECTION PER THE GUIDELINES AND ACKNOWLEDGED BY THE DRC IN A COMMENT BELOW.
 - THE FLOOR TO FLOOR HEIGHT AT THE TOWPATH LEVEL IS DICTATED BY PHYSICAL SITE CONSTRAINTS.

Massing

- The intent of the guidelines is to avoid creating an inauthentic facsimile of the historic urban form - instead the intent is to develop characteristics of historic precedents at Canal Side. Lloyd Street Front Door Entrance 01, is not as active and welcoming as Front Door 02 on the Mercantile Bridge elevation.
 - THE MUSEUM ENTRANCES REFLECT DIFFERENT VISITOR PROFILES (LLOYD – GENERAL VISITOR, BRIDGE – SCHOOL GROUPS) AND HAVE BEEN APPROVED BY THE MUSEUM.

- Note later comments on the rooftop skyline. The arch on the Lloyd street side is weak because it is flat against the building. It is recommended that this projects out to at least the line of the canopies for weather protection for the children using this facility.
 - THE ARCH ON LLOYD STREET IS DESIGNED WITH MUSEUM INVOLVEMENT. IT IS EXPECTED THAT A MAJOR SIGNAGE ELEMNTN WILL BE APPLIED AT THIS ENTRANCE. THE DETAILING OF THE ARCH REFLECTS THIS FUTURE APPENDAGE.

Build-to Lines

- There is a large setback on the Marine Drive side of the facility. However, this parcel is not specifically addressed in the design guild lines. The Lloyd street side and canal side of the facility have great street walls.
- See comments regarding Streetscape.

Height Limits

- The facility is approximately 60' high and is within the height limit of other parcels along marine drive.

Circulation

- Circulation within the building appears very cut up by what appears to be diagonal bracing and the bracing would be very unfortunate in front of the glass wall to the roof terrace.
 - THE MUSEUM WILL BE INTEGRATING AND EXPOSING IN CERTAIN AREAS (I.E., REVEALING) THIS DIAGONAL BRACING AS A HONEST WAY TO EXPRESS THE STRUCTURE AS PART OF THE INTERIORS DESIGN.
- Is diagonal bracing the only way to stiffen the structure?
 - YES – THE DIAGONAL BRACING IS THE ONLY WAY TO STIFFEN THE NEW STRUCTURE Laterally (i.e., CODE – WIND/SEISMIC) LOCATED ABOVE THE EXISTING ECHDC SUBBASEMENT CONDITIONS.
- It is our recommendation that the base of the Entrance 01 pushes to reflect either historic precedent or a playful "contemporary" interpretation of an industrial mercantile vocabulary. The design team recommends further inquiry in the design of exterior lighting to reflect the same.
 - CIRCULATION DESIGN GUIDELINES RELATE SPECIFICALLY TO EXTERIOR CIRCULATION. THE INTERIOR CIRCULATION COMMENTS/QUESTIONS ABOVE ARE THE RESPONSIBILITY OF THE MUSEUM.

Streetscape

- There is not enough detail to comment of streetscape materials. Approval should be contingent of a future review of streetscape materials.
 - SUFFICIENT SITE DETAILS WERE PROVIDED FOR REVIEW.
- Sheet C012 - Not sure what the base conditions are for curbs, sidewalk, or road, but we strongly recommend the the "alternate bid" be the standard granite curb, exposed medina aggregate, sandstone pavers, & sandstone cobble pavers for Lloyd Street.

- LLOYD STREET MATERIALS ARE REFLECTIVE OF HISTORIC MATERIALS (I.E., GRANITE CURB, SANDSTONE COBBLESTONES, AND EXPOSED MEDINA AGGREGATE SIDEWALK TO MATCH OTHER CITY STREET SIDEWALKS).
- Why brushed concrete stairs? We recommend using the canal side standard of dark granite.
 - SEE RESPONSE ABOVE.
- No indication of stair riser & tread dimensions. On these exterior stairs I would strictly enforce a maximum rise of 6".
 - ALL STAIR RISER MEETS ADA REQUIREMENTS.
- A101 Tow Path Plan - watch for door swings into a very busy corner. It could be the busiest corners at Canal side.
 - DOOR SWINGS ARE TYPICAL OF RETAIL/COMMERCIAL ESTABLISHMENTS.
- West Elevation – The canopy should extend another bay to the south.
 - THIS WILL BE REVIEWED FURTHER BY ECHDC AND THE MUSEUM.
- Guidelines call for a two story base.
 - THE STREET LEVEL "BASE" IS 17 FT. IN HEIGHT, WHICH IS NEARLY A "TWO-STORY" BASE IN HISTORIC TERMS. AT THE SOUTHERN END OF THE BUILDING, THE SITE GRADE DROPS AND THE "BASE" INCREASES TO NEARLY 23 FT.
- Please provide sections A-306, 1-A300, 1-A314 to fully understand the buildings edge at the canal.

Parking

- N/A

Structural parking

- N/A

Access and entrances

- Need better definition of curbing & cuts on south side of building.
 - SUFFICIENT SITE DETAILS WERE PROVIDED FOR REVIEW.

Refuse collation

- The general service street is along Marine Drive per the guidelines. There is already service entry located at harbor center along Marine Drive, so there is precedent for this.
- All trash stored in building & taken outside the morning of pick up. Make every attempt to now have trash sitting outside overnight.

Architectural features

- The metal composite paneling shown along the Lloyd street entrance will look very corporate and does not reflect the historic nature of the area.
 - THE MASTER ARCHITECT AND ARCHITECT OF RECORD ARE FOLLOWING A PHILOSOPHICAL DECISION MADE BY ECHDC. INTERPRETATION OF HISTORIC ELEMENTS ARE FINISHED IN HISTORIC OR SIMILAR MATERIALS. MODERN ELEMENTS

(VEHICULAR REFUSE SURFACE, MUSEUM BLDG STAIRS, RAMPS, ETC.) ARE FINISHED IN SIMPLE, MODERN MATERIALS AND FINISHES.

- From our understanding the metal composite arch of the main entry, doesn't allow coverage from rain/snow/sun while groups are gathering to enter the building. A covered awning would also serve as a visual cue.
 - SEE RESPONSE ABOVE.
- Rubbed finish concrete surfaces should not be allowed at the base of the building as per sheet A201 West Elevation.
 - THE MASTER ARCHITECT AND ARCHITECT OF RECORD ARE FOLLOWING A PHILOSOPHICAL DECISION MADE BY ECHDC. INTERPRETATION OF HISTORIC ELEMENTS ARE FINISHED IN HISTORIC OR SIMILAR MATERIALS. MODERN ELEMENTS (VEHICULAR REFUSE SURFACE, MUSEUM BLDG STAIRS, RAMPS, ETC.) ARE FINISHED IN SIMPLE, MODERN MATERIALS AND FINISHES.
- Light fixtures at both entrances should be bold & very special, and much larger.
 - THIS WILL BE REVIEWED FURTHER BY ECHDC AND THE MUSEUM.
- The south elevation with the ribbed metal is weak. The window pattern doesn't contribute to Canalside. This would have been a perfect opportunity for a very glassy elevation showing off the guts of the building.
 - THE SOUTH FACE IS NOT AN HISTORIC FAÇADE BUT IS A "SECTIONAL CUT" THROUGH THE LOCATION OF A FORMER BUILDING. INTERPETING THIS "CUT" WITH A METAL FINISH AND CONTEMPORARY WINDOWS MAKES THIS CLEAR. THE SIMPLE TREATMENT ALSO IS CONSCIOUS OF THE FACADES LOCATION UNDER THE SKYWAY AND ITS USE AS AN AREA OF REFUSE.

Edges

- How is the sharp corner of the building along Maine Drive detailed? Please provide plan details at 1"=1' scale

Bases

- We would like to see more detail in the base of the building on the Lloyd street elevation. There is an opportunity for the history of the area to be introduced at this level.
 - THIS WILL BE REVIEWED FURTHER BY ECHDC AND THE MUSEUM.
- Guidelines call for a two story base.
 - THE STREET LEVEL "BASE" IS 17 FT. IN HEIGHT, WHICH IS NEARLY A "TWO-STORY" BASE IN HISTORIC TERMS. AT THE SOUTHERN END OF THE BUILDING, THE SITE GRADE DROPS AND THE "BASE" INCREASES TO NEARLY 23 FT.

Storefront and Retail facades

- There is not enough detail provided to evaluate. The design of the exterior doors should be special. Appear to standard models from a suburban drug store. How do these contribute to the character of Canalside?
 - THE DOORS SHOWN WERE SELECTED BY THE MUSEUM. THIS WILL BE REVIEWED FURTHER BY ECHDC AND THE MUSEUM.

- No vestibule or air-lock at the two main entrances?
 - VESTIBULES ARE LOCATED INTERIOR OF THE DOORS AND WILL BE PART OF THE INTERIOR BUILD-OUT OF THE MUSEUM'S SCOPE OF WORK.

Corners

- How is the sharp corner of the building detailed? This might be the ideal place for a large vertical sign the height of the building elevation. **The Guideline** "Building Corners provide an opportunity for establishing architectural emphasis. Perhaps this is why the graphic signage on the Adaptive re-use addition facade is shown by the museum. Suggestion : The architecture wraps the corner.
 - THE RETAIL SPACE ALONG THE EAST FAÇADE WRAPS THE CORNER ONTO THE SOUTH FAÇADE. THE REST OF THE SOUTH FACE IS DEDICATED TO REFUSE COLLECTION PER THE GUIDELINES AND ACKNOWLEDGED BY THE DRC IN A COMMENT BELOW.

Cornices

- The main building loses its cornices for a metal coping which seems to be an afterthought. The main building cornices should be more pronounced.
 - HISTORIC PRECEDENTS INDICATE THAT THE CANALSIDE ERA BUILDING'S COMMON CORNICES WERE SIMPLE AND LESS PROFILED AND PROJECTED. THIS WILL BE FURTHER REVIEWED FURTHER BY ECHDC AND THE MUSEUM.
- Not sure why the north building has a metal cornice. Not enough detail to evaluate this. We enjoy the corbeled simpler brick cornice but object to the aluminum roof edge coping. Why not a clay camelback coping?
 - THE NORTH END OF THE OVERALL COMPOSITION FOR THE MUSEUM'S IS MEANT TO EXPRESS TWO BUILDINGS CONTIGUOUS IN NATURE TO EACH OTHER BUT PART OF THE SAME FAMILY AND ERA OF BUILDING. THE METAL CORNICE INCORPORATES THE LIGHTING REQUIRED FOR THE CANAL SYSTEM. TRADITIONAL CLAY CAMELBACK COPINGS ARE NOT APPROPRIATE TO THE SKIN OF THE BUILDING'S RAINSCREEN TECHNOLOGY NOR COMPATIBLE TO THE MUSEUM'S ADDITIONAL GLASS GUARD RAIL THAT WILL SIT ONTOP OF THIS CORICE LOCATIONS.
- The metal panels above the glazing to the roof terrace are weak.
 - THE METAL PANELS ARE MEANT TO WEAVE IN A MODERN EXPRESSION INTO THE CONTEXT OF A FORMER CANALSIDE ERA MERCANTILE BUILDING. THE SIMPLICITY OF THE METAL PANELS AND THE SIMPLICITY OF THE GLASS CURTAIN WALL SYSTEM ELEGANTLY EXPRESS AND PROVIDE AN APPROPRIATE BACKDROP (I.E., STAGE) FOR THE WHIMSICAL NATURE OF CHILDREN'S MUSEUM'S CONTENTS.
- Light fixtures on the metal cornice? No detail & what are they illuminating? Will these cause glare, hot spots, & general light pollution?
 - THE METAL CORNICE INCORPORATES THE LIGHTING REQUIRED FOR THE CANAL SYSTEM.
- Material no. 24 - metal coping / fascia is inappropriate for this building. Clay camelback would finish it beautifully. The coping appears oversized and the usually show oil canning.
 - REFER TO PREVIOUS COMMENT ON THE CLAY CAMELBACK SYSTEM MENTIONED ABOVE. THIS WILL BE REVIEWED FURTHER BY ECHDC AND THE MUSEUM.

Canopies

- Same note as above: From our understanding the metal composite arch of the main entry, doesn't allow coverage from rain/snow/sun while groups are gathering to enter the building. A covered awning would also serve as a visual cue.
 - THE ARCH ON LLOYD STREET IS DESIGNED WITH MUSEUM INVOLVEMENT. IT IS EXPECTED THAT A MAJOR SIGNAGE ELEMENT WILL BE APPLIED AT THIS ENTRANCE. THE DETAILING OF THE ARCH REFLECTS THIS FUTURE APPENDAGE.
- Not enough detail to evaluate. Why three different types of canopies? Standing seam, sunshade type (does this provide weather protection?)
- And a glass system. Details would be helpful to evaluate.
 - PER THE DESIGN GUIDELINES "VARIETY AND NON-REPETITIVE DESIGN ARE DESIRED. CANOPIES CAN BE CONSTRUCTED OF A VARIETY OF MATERIALS INCLUDING BOTH FABRIC AND METAL."

Skyline

- Visual interest is needed at the rooftop to fully comply with the design guidelines.
 - HISTORIC PRECEDENTS OF CANAL ERA BUILDINGS HAVE CHIMNEYS AND STAIR EXTENSIONS ABOVE THE ROOF LEVEL AND ARE COMMON. THE THREE CHIMNEYS AND THE SINGLE ENCLOSED SCREENING EVOKE THIS THEMATIC UNDERSTANDING OF THIS CONTEXT. THE ROOFTOP TERRACE IS LOCATED ALONG THE CANALS TO "TO TAKE ADVANTAGE OF WATERFRONT VIEWS" AND THE MECHANICAL AND HVAC EQUIPMENT IS SCREENED OR VENTED INTERNALLY AS PER THE DESIGN GUIDELINES.

Materials

- The metal composite paneling shown along the Lloyd street entrance will look very corporate and does not reflect the historic nature of the area. The color will be very important.
 - THE MASTER ARCHITECT AND ARCHITECT OF RECORD ARE FOLLOWING A PHILOSOPHICAL DECISION MADE BY ECHDC. INTERPRETATION OF HISTORIC ELEMENTS ARE FINISHED IN HISTORIC OR SIMILAR MATERIALS. MODERN ELEMENTS (VEHICULAR REFUSE SURFACE, MUSEUM BLDG STAIRS, RAMPS, ETC.) ARE FINISHED IN SIMPLE, MODERN MATERIALS AND FINISHES.

Building Materials and color.

- Material 07 & 08, stone headers & lug sills should be real cut stone & not imitation materials. The brick system, while I consider it questionable, it is at least a real clay product.
 - THE LUG SILLS AND HEADERS AT NOTED LOCATIONS ARE A GLASS FIBER REINFORCED CONCRETE SYSTEM PRODUCT THAT HAS A STONE OUTER SURFACE THAT IS APPROX. 1/8" TO 1/4" THICK. THIS PRODUCT IS INTEGRATED WITH THE LIGHTNESS OF THE SYSTEM NEEDED FOR THE ENVELOPE WHILE MAINTAINING AN APPROPRIATE AESTHETIC FOR THE MERCANTILE FACADES. THIS WILL BE REVIEWED FURTHER BY ECHDC AND THE MUSEUM.
- Recommend a dark painted color on the punched windows. Is there rowlock coursing above the cast stone headers? It is usually one of the other? This is on the north face of the building.

- THIS WILL BE REVIEWED FURTHER BY ECHDC AND THE MUSEUM.

Glass and fenestration

- The DRC needs to see a sample of the spandrels glass that is proposed on the east elevation.
- The design guidelines require the building glass be clear. The glass is clear, but is then treated with a metallic low-e coating, which helps energy performance, but in my opinion reduces the light transmittance value, but more importantly adds to the reflectivity of the glass from the outside during the day, and adds to the reflectivity at night from the inside. The intent of the guidelines is to promote TRANSPARENCY. A good mock up might prove otherwise to the DRC.
 - A SAMPLE HAS BEEN SHARED AND REVIEWED. THE GLAZING HAS PHENOMENAL PERFORMANCE WHILE BEING THE MOST TRANSPARENT NON-REFLECTIVE PRODUCT AVAILABLE. IT'S PERFORMANCE IS REQUIRED TO MEET ENERGY EFFICIENCY.
- Note that the colored rendered elevations do not match the technical elevations.
 - THIS STATEMENT IS INCORRECT.

Signage

- This approval would be contingent of a future review of proposed signage. The west and south elevations lacked the visual delight and invention mentioned in the design guidelines. The entrance at door 01 also lacked the visual delight. As mentioned above: the corner on Marine Drive might be a place for for a large vertical sign the height of the building elevation.
 - THE MUSEUM WILL SUBMIT INDEPENDENTLY FOR EXTERIOR SIGNAGE.

Lighting

- This approval would be contingent of a future review of proposed lighting.
- The choice of lighting could be more playful or historic. As mentioned above "historic precedent" or a playful "contemporary" interpretation of an industrial mercantile vocabulary would offer a stronger design solution. Perhaps the choice of the fixture is oversized rather than undersized.
- Not nearly enough information to evaluate. Down lighting should be used sparingly & with effective glare control.
- Previous reviews included a presentation from a lighting consultant. Very important to prove a cohesive feel & character of lighting for all of Canalside. Not always in terms of fixture type, but color rendering index & color temperature in degrees Kelvin.
 - THIS WILL BE REVIEWED FURTHER BY ECHDC AND THE MUSEUM.



Sustainability guidelines

- No comment.

Energy

- The metal ribbed paneling on the south side of the facility will absorb a lot of heat. Has the use of this paneling during the summer months been taken into consideration? Further attention is needed regarding the color of this paneling.
 - THE BUILDING ENERGY MODEL IS COMPLETE AND A LEED SILVER RATING IS EXPECTED.

Indoor environmental quality

- There is not enough detail provided to comment on this.
 - THE CORE AND SHELL BUILDING IS EXPECTED TO RECEIVE A LEED SILVER RATING.

Water Management

- There is not enough detail provided to comment on this.
 - THE MUSEUM IS RESPONSIBLE FOR WATER MANAGEMENT. THE CORE AND SHELL BUILDING IS EXPECTED TO RECEIVE A LEED SILVER RATING.

Outdoor environment and site construction

- There is not enough detail provided to comment on this.
 - SUFFICIENT SITE DETAILS WERE PROVIDED FOR REVIEW.

We appreciate the opportunity to submit our Review Commentary of DRC's Recommendations and if you should have any questions or comments feel free to contact me at 716-662-2200.

Regards,

FONTANESE FOLTS AUBRECHT ERNST, ARCHITECTS, P.C.

A handwritten signature in blue ink, reading "Philip S. DiNicola".

Philip S. DiNicola, R.A.,
Principal